

# Marion St. Bridge

**Jenny A. Durkan**  
Mayor

**Samuel Asefa**  
Director, OPCD

**John Savo**, Chair

**Ben de Rubertis** Vice Chair

**Justin Clark**

**Rikerrious Geter**

**Rachel Gleeson**

**Laura Haddad**

**Brianna Holan**

**Mark Johnson**

**Rick Krochalis**

**Ross Tilghman**

**Michael Jenkins**  
Director

**Valerie Kinast**  
Coordinator

**Aaron Hursey**  
Planner

**Matthew Allen**  
Administrative Staff

**Office of Planning and  
Community Development**  
**Seattle City Hall**  
600 4th Avenue, 5th Floor  
Seattle, WA 98124

**TEL** 206-684-0435  
**FAX** 206-233-2784  
seattle.gov/designcommission

## Commissioners Present

John Savo, Chair  
Ben de Rubertis, Vice Chair  
Justin Clark  
Rikerrious Geter  
Laura Haddad  
Brianna Holan  
Mark Johnson  
Rick Krochalis  
Ross Tilghman

## Commissioners Excused

Rachel Gleeson

## Project Description

Marion Street Bridge provides an elevated connection between downtown, Colman Dock, and the waterfront. A portion of the existing bridge will be replaced as part of the removal of the Alaskan Way Viaduct and reconstruction of Alaskan Way. The project is a part of the Waterfront Seattle development and is being designed in cooperation with the Washington State Department of Transportation (WSDOT). The bridge will be constructed out of concrete and will include a 16 ft. wide pathway, lighting, and pedestrian railing. The bridge will be constructed in two phases. Phase I includes the design and construction of the bridge crossing Alaskan Way from Colman Dock to Western Ave. Phase 2 will include the design and construction of a pedestrian pathway above Marion St. from 1st Ave to Western Ave.

## Meeting Summary

This was the Seattle Design Commission's (SDC) third review of the Marion Street Bridge project. The purpose of this meeting was to review the design development (90% design phase) for the project. After the presentation and discussion, the SDC voted 8-1 to approve the design development phase with several recommendations.

## Recusals and Disclosures

None

**January 3, 2019**

9:00 - 10:30 am

**Type**

CIP

**Phase**

90% - Design Development

**Previous Reviews**

[10/4/18](#), [8/2/18](#)

**Presenters**

Miguel Rosales  
Rosales Partners

Kit Loo  
SDOT

**Attendees**

Angela Brady  
Office of the Waterfront

Don Nguyen  
HDR

Steve Pearce  
Office of the Waterfront

Genevieve Rucki  
WSF

Alec Williamson  
WSDOT

Stephen Wilson  
SDOT

David Yuan  
NBBJ - WSF

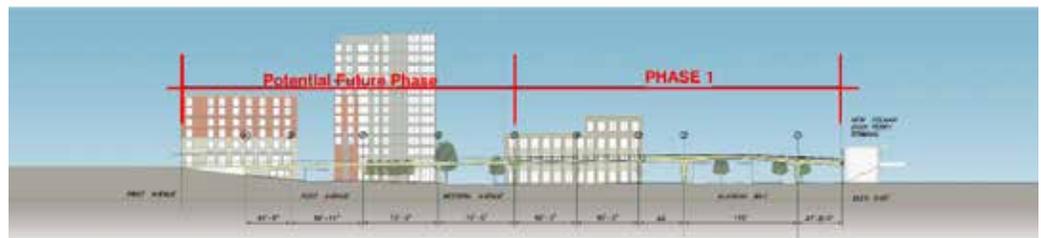


Figure 1 Plan and section of proposed bridge design

**Summary of Presentation**

Miguel Rosales, of Rosales Partners, and Kit Loo, of SDOT presented the design development phase for the Marion Street Bridge project. The project team provided a brief update on the overall project timeline followed by a detailed presentation on the updated design. Overall updates include changes to the proposed columns, lighting, and temporary transition near the east side of the bridge. The team maintained popular design elements like consistent lighting throughout the varying widths of the structure, horizontal curvature, and minor grade change in the east-west slope. The team is continuing to work with Washington State Ferries and Colman Dock to coordinate materials where the bridge joins the ferry terminal.

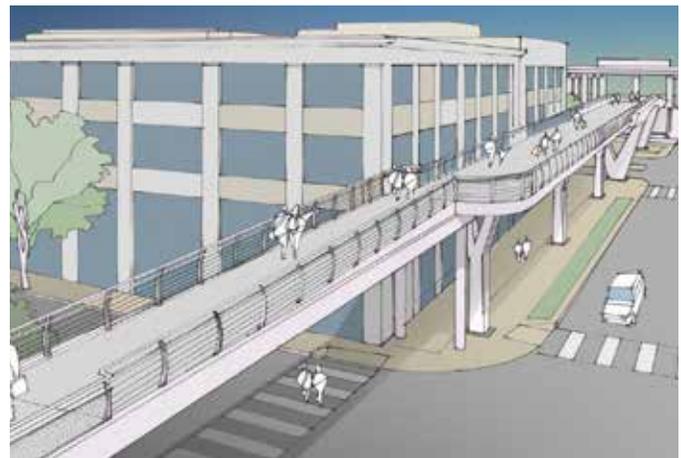


Figure 2: proposed design of temporary transition (top), updates to the proposed lighting scheme (bottom)

Design updates to the temporary transition ramp near Western Avenue has been redesigned to include a rounded, bulb-like shape to create a space that can serve as a viewing platform, while eliminating an abrupt dead-end for pedestrians travelling from the ferry terminal to 1st Ave. The lighting has

evolved into using spotlights along the bridge columns as well as on the underside of the bridge handrails to accentuate the lines of the structure and illuminate the walkways. (*see figures 1 & 2 for more detail*)

The design team hopes to continue coordination with the Post Apartments and the developers of 815 Western Avenue to include additional access to the bridge.

### Agency Comments

**None**

### Public Comments

**Claire Enlow**, Local reporter, is concerned with how the proposed bridge will connect with the adjacent ferry terminal. She requested to see information on the topic.

### Summary of Discussion

The Commission organized its discussion around the following issues:

- Concept development
- Access, mobility, and circulation
- Lighting and artwork
- Materials and sustainability

#### *Concept development*

The SDC commended the project team for their responsiveness to previous design issues related to lighting, column shape, and handrail details. The commission expressed concern with the east and west transition points, including the lack of details describing the materials and function of the transition points at the east and west end of the pedestrian bridge. The Commission strongly recommended the project team provide more details about the proposed materials and function of both transition points. The Commission also expressed concern with the proposed transition between the bridge and Colman Dock. Commissioners agreed the current proposal for the design of the transition was not adequate in providing a sense of arrival for pedestrians using the bridge to access Colman Dock. The SDC then recommended the project team continue to work with Colman Dock to create a partnership so the ferry terminal can serve as a point of arrival for pedestrians using the bridge.

The SDC then expressed concern over the current proposal for drainage and water management. Although commissioners understand the project team's desire to create bridge elements that were as elegant as possible, they strongly recommended project team think of creative solutions to solve issues related to the location of drainage. Specifically commissioners recommended the project team consider the integration of drainage within the columns, so that the appearance of any drainage features are well integrated with the building concept and design details.

The SDC then discussed issues around future phasing, funding, and temporary transitions. The Commission continued to raise concerns over constructing the project in two phases. Commissioners are concerned that if the full project is not constructed at one time, then the future phase might not be constructed. The SDC strongly recommended the project team provide clarification regarding timing of funding for the future phase. The Commission also recommended the project team continue to explore funding opportunities to build the project in one phase.

The Commission commended the project team for responding to previous concerns related to the design of the transition between the proposed bridge and existing pedestrian pathway. Specifically, commissioners agreed with the proposed curve guardrail design at the location where Phase 1 will terminate at Western Avenue. Several commissioners are worried that the curved railing might imply that this is a permanent transition, while other commissioners questioned why so much time and funding was being allocated to a temporary section. The SDC then recommended the project team reuse the guardrail elements along the temporary guardrail for the future phase.

#### *Access, mobility, and circulation*

The SDC commended the project team for the overall access and circulation related to the pedestrian bridge.

Specifically, commissioners appreciated the gentle horizontal curve in the bridge over Alaskan Way, which improved sight lines along Alaskan Way. The Commission continued to voice their support for constructing the full project in one phase as well as coordinating with the property owner at the Commuter Building location to include additional vertical circulation. The Commission strongly encouraged the project team to integrate materials near the east and west transition with materials used with connecting facilities.

#### *Lighting and artwork*

The SDC agreed that the project team should not attempt to force art within the design if there is not an appropriate location. The commission identified the east and west transition areas at Western and Colman Dock as potential locations for art, if transitions cannot match the design and materials used on the connecting facilities. Several commissioners also identified the stairway as an opportunity for art. The commission also recommended the project team explore using texture on the columns so they might serve as an artist piece/sculpture within the landscape.

The SDC appreciated the overall direction of the proposed design and location of lighting elements as well as the proposed lighting program (the use and timing of color changing fixtures). Commissioners appreciated that the project team was consulting with a lighting designer and suggested the team utilize the color changing lighting option only during appropriate events throughout the year. The commission then recommended the project team explore the newest technology for pedestrian lights, specifically as it relates to including warmer white lights and RGB-W fixtures.

#### *Materials and sustainability*

The SDC recommended the project team use sustainable materials with longer life cycles. The commission then reiterated their preference to integrate the proposed drainage within the columns.

### **Action**

The Commission thanked the project team for its presentation on the design development phase for the Marion Street Bridge Project. Overall the Commission appreciated the project team's responsiveness to previous issues related to lighting, column shape, and handrail details as well as adequately addressing accessibility and the slope and horizontal angle of the bridge from a functional and visual perspective. The SDC voted, 8-1, to approve the design development phase of the Marion Street Bridge project with the following recommendations:

1. Continue to think about proposed materials and overall function of transition at the east and west end of the bridge
2. Continue to work with Colman Dock to create a partnership so they can be a point of arrival and receiving partner for the pedestrians using the pedestrian bridge
3. Think creatively to solve issues related to the location of drainage elements on west end of the pedestrian bridge, including the potential for integration of drain into column
4. Provide clarification of timing of funding for future phase and continue to explore funding opportunities to build the project in one phase
5. Explore reusing guardrail elements in temporary walkway for future phase
6. Explore providing an opportunity for art at both transition points if transition cannot match materials used on the connecting facilities
7. Explore newest technology for pedestrian lights, specifically to include warmer white light
8. Explore using texture on columns to provide structure in the landscape
9. Use sustainable materials with longer lifecycles
10. Hold a subcommittee meeting to present the changes near Colman Dock concerning the railing, expansion joint, and drainage.

The following are statements from commissioners who voted against the design development phase for the Marion Street Skybridge project:

**John Savo-** I applaud what the project team is doing overall, but It is important to emphasize more than what was stated about future funding. The need to complete the project in one phase or to at least have a timeline for when it the future phase will be constructed should be strongly considered.